

## Operating Manual

### TRL-3D 20 Three-Sided Dumper



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## Introduction

Dear customer! Dear customer!

Thank you for purchasing a product of matev GmbH, we appreciate your trust. You have acquired a quality product, if contrary to expectations you should have problems with the device, contact your responsible sales partner.



Before using the trailer for the first time, please read this operating manual carefully and conscientiously all the way through. The manual describes the handling of the trailer in detail and will support you in working safely and effectively.

For damage that occurs due to improper use, we will not accept any warranty claims.

Variants may be described in the manual that do not match the scope of delivery of your trailer.

Please enter the last five digits of the vehicle identification number in the manual. This information will be helpful in communication if there is a service case.

Matev GmbH Nürnberger Straße 50 90579 Langenzenn	<b>matev</b>	CE
Matev GmbH R2a e1*167/2013*00276*00 W093D2126L2M <input type="text"/>		
		T-1 T-2 T-3
2500kg	B-1	— — —
A-0 400 kg	B-2	— — —
A-1 2500 kg	B-3	— — —
A-2 kg	B-4	— — —
A-3 kg		

**The matev products are subject to change in the interest of technical progress. All information, illustrations, and technical specifications represent the latest status at the time this manual was published. The manufacturer reserves the right to make changes at any time in the interest of technical progress.**

Regards

matev GmbH

## 1 About this operating manual

Prior to first use, read the manual carefully and conscientiously all the way through.

Keep this manual where it is easily accessible. This will enable you to refer to important information and handling instructions as needed.

At transfer of the trailer the owner of the trailer receives instruction in the operation and maintenance of the trailer from the owner's sales partner.

The owner must ensure that operating and maintenance personnel have been instructed in full scope in the operation and maintenance of the trailer.

Listings with bullet points are marked as follows:

- Text
- Text
- Text . . .

Handling instructions are marked in the sequence, in which they should be executed, as follows:

1. Text
2. Text
3. Text . . .

## 2 Technical data

Description		
Type	TRL-3D 20	
Chassis no.: <sup>1</sup>	W093D2126L2M...	
Number of axles	1 axle	
Number of braked axles (over-run device)	1 axle, 2 wheels	
Brake	Overrun device	
Dimensions		
Length incl. towing device	3400 - 3470	
Width	1390 - 1430	
Height	1130 - 2015	
Weight		
Empty weight (depends on the equipment)	390 kg – 464 kg	
Perm. axle load	2500 kg	
Perm. Tongue load	400 kg	
Tire combinations		
1	Tire dimensions	205 R 14C PR8 109 P
2	Tire dimensions	10.0/75-15.3 PR10 123 A8/129 A6
Towing device		
1	Manufacturer – type	ADR – RZ080
2	Manufacturer – type	ADR – RZ080C
3	Manufacturer – type	BPW – AVE 2000
Drawbar eye		Ø40, DIN 74054 / ISO 8755
Rear axle		
1	Manufacturer – type	ADR – A60LG6FSN (1000)
2	Manufacturer – type	BPW – DO 55.53.381.020

Table 1: Technical data

<sup>1</sup> Please enter

## 3 Safety notice

Guidelines and instructions that you must comply with when dealing with machines are presented in this section.

### 3.1 Intended use



#### **Danger!**

**To avoid misuse, also read the section 4 Equipment all the way through.**

- The multifunction trailer is used convey various transport goods. Transporting persons or animals is prohibited.
- Axle loads, tongue loads, and permissible gross weight specified in Table 1 must be complied with.
- The tractor must be equipped with a suitable pin coupling Ø4 (DIN 11025, DIN 11028 or DIN 74051).
- Using the load bed as a lift platform is prohibited.
- Driving with lifted or unlocked load bed is prohibited.
- Maximum speed is 40 km/h
- The trailer must be approved for road traffic.
- The main inspection, i.e. technical test, must be performed at regular intervals. Comply with statutory regulations.
- Driver's license: Depending on the country, an appropriate driver's license may be required for driving with trailer.

Non-intended use causes:

- Danger of injury to the operator or third parties
- Damage to the tractor and the implements
- Environmental damage

### 3.2 Instructions for the owner

#### 3.2.1 Qualifications of personnel

The trailer may only be operated, serviced, and maintained by persons who are qualified, as follows:

- They must be at least 18 years old.
- They must have received instruction.
- They must have read and understood this operating manual.
- They must have been instructed concerning the relevant safety regulations (accident prevention).

The relevant accident prevention regulations and the other generally acknowledged occupational health and safety regulations and road traffic regulations must be complied with at all times.

#### 3.2.2 Instruction

At transfer of the trailer the owner receives instruction in the operation and maintenance of the trailer from the owner's sales partner.




The owner must ensure that operating and maintenance personnel have been instructed in full scope in the operation and maintenance of the trailer.

### 3.2.3 Accident prevention

The safety regulations and accident prevention regulations are legally regulated in every country. The owner of the machine is responsible for compliance with the valid regulations.

### 3.3 Presentation of the general safety notices

The safety notices used in the text employ pictograms and signal words, which in combination indicate the severity of the risk to be expected.

	<b>Danger!</b> <b>Dangerous situations that can directly result in severe or fatal injuries.</b>
	<b>Warning!</b> <b>Dangerous situations that can possibly result in severe or fatal injuries.</b>
	<b>Attention!</b> <b>Dangerous situations that can possibly result in minor injuries or material damage.</b>
	<b>Note!</b> <b>Recommendations or information for the operator.</b> <b>Read this notice. It facilitates your work.</b>

### 3.4 Warning signs

Warning signs indicate hazards that cannot be countered through protective measures.

To avoid severe or fatal injuries the warning signs must always be obeyed.

#### 3.4.1 General warning signs

In this section the non-specific, generally-valid warning signs are explained.



**Read the operating manual and comply with the safety notices.**



**Prior to performing maintenance and repair tasks, turn off the engine and remove the key.**





**Injuries due to fluid escaping under high pressure.  
Comply with the notice in the operating manual.**



**Never reach into the crushing hazard danger zone if the parts there can move.**



**Presence in the danger zone is only permitted when the safety supports are inserted.**

### 3.4.2 Machine-specific warning signs

Machine-specific warning signs are explained in this section.



**Prior to uncoupling, secure the trailer with wheel chocks.**



**Do not enter the dumping area if the load bed is lifted.**



**Never dump the load bed on a soft substrate or on a slope.**

### 3.5 Safety notices and safety devices



#### **Danger!**

Each time before starting to tow the trailer, ensure that all described safety notices and safety devices are completely present and functional. Do not operate the trailer if safety notices or safety devices are missing or damaged.

#### 3.5.1 Safety device

- Breakaway cable between tractor and the handbrake of the trailer.



Breakaway cable

Fig. 1: Breakaway cable

- 2 reflectors white and wheel chocks on front panel



Fig. 2: Reflector, wheel chock

- Left and right side panel, each with 2 yellow reflectors



Fig. 3: Left side panel, reflector



Safety notice

- Possibly with optional lighting



Fig. 4: Optional lighting

- Lowering safeguard and safety cable for tilt limitation

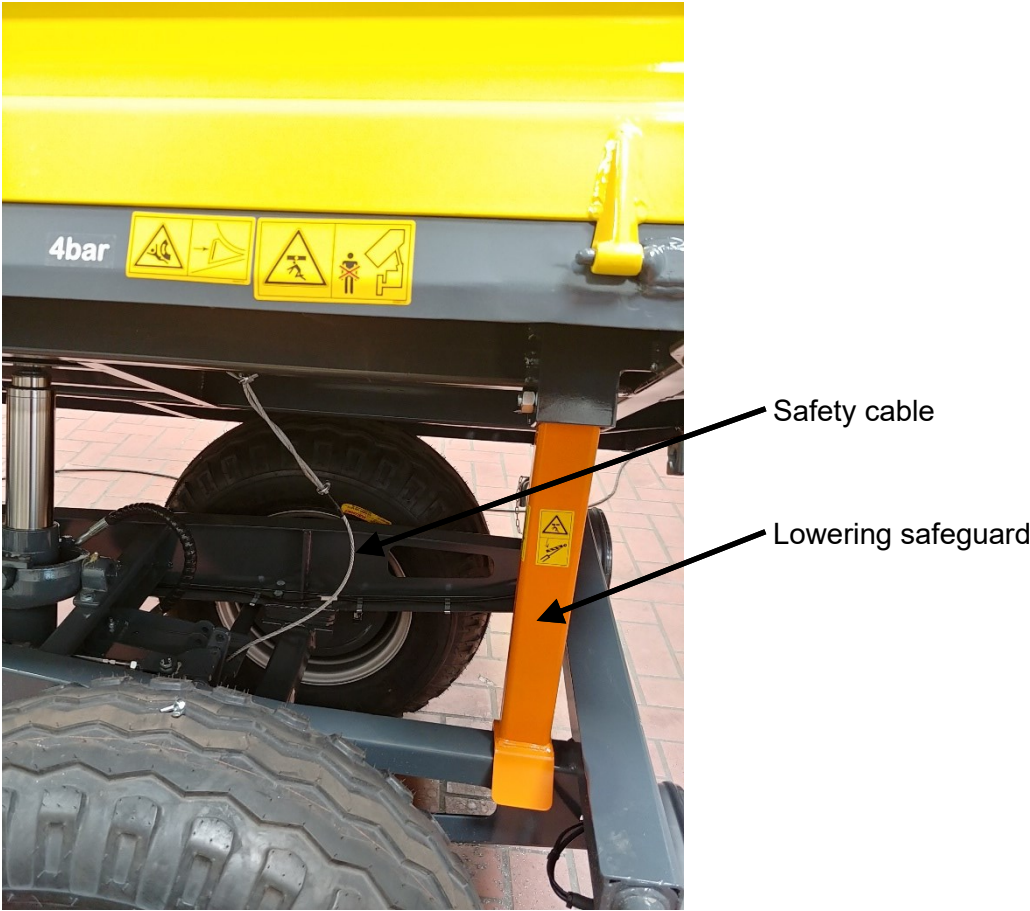


Fig. 5: Lowering safeguard

3.5.2 Safety stickers

- On the drawbar



Fig. 6: Safety stickers on the drawbar

- Drawbar left



Fig. 7: Safety stickers and rating plate, drawbar left



Safety notice

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- Drawbar right

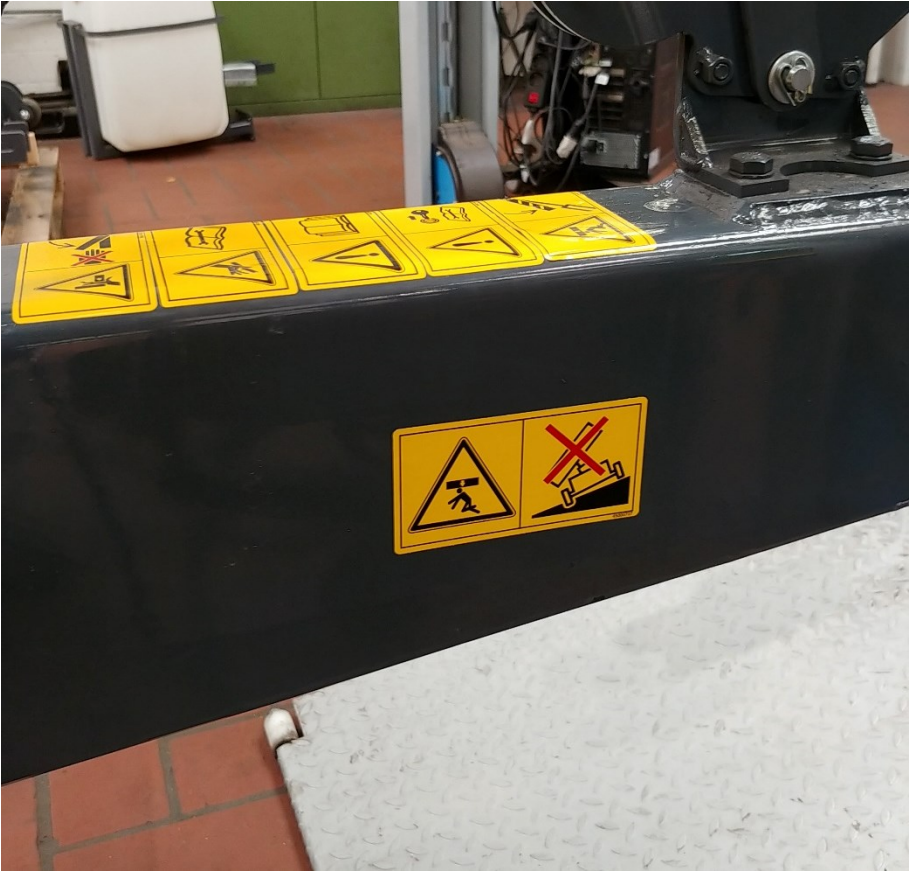


Fig. 8: Safety stickers drawbar right

- Safety stickers trailer right



Fig. 9: Safety stickers trailer right

- Safety stickers trailer rear



Fig. 10: Safety stickers trailer rear

Safety notice

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- Safety stickers trailer left



Fig. 11: Safety stickers trailer left

- Safety stickers lowering safeguard, trailer left



Fig. 12: Safety stickers lowering safeguard



## 4 Equipment

In the standard situation the trailer is delivered only partially assembled. Pre-assembly is possible for an additional charge.

### 4.1 Coupling

Coupling on the towed vehicle in accordance with DIN 74054 / ISO 8755, Ø40 suitable for coupling on the towing vehicle DIN 11025, DIN 11028 Form A, B, C, and DIN 74051 Part 1.

### 4.2 Drawbar

The drawbar can be mounted approx. 535, 565 or 620 mm above the ground.



**Danger!**  
**Never change the height of the drawbar on your own. Always commission a specialized company.**

### 4.3 Support wheel

The trailer is equipped with a hinged support wheel, which is height-adjustable via a spindle.

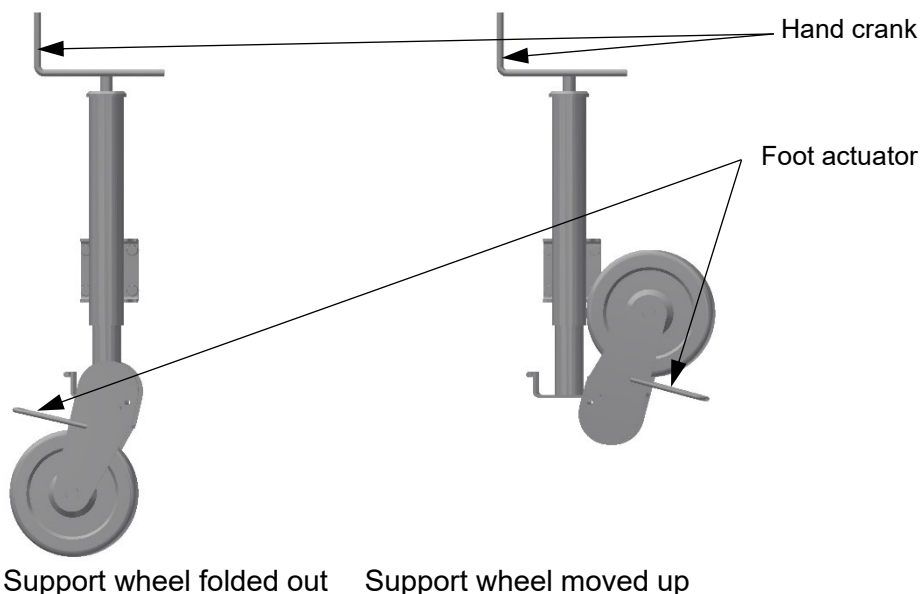


Fig. 13: Support wheel



**Warning!**  
**After coupling the trailer to the towing vehicle and before starting to drive, lift up the support wheel.**



**Attention!**  
**Only actuate the support wheel with your foot.**

## 4.4 Brake

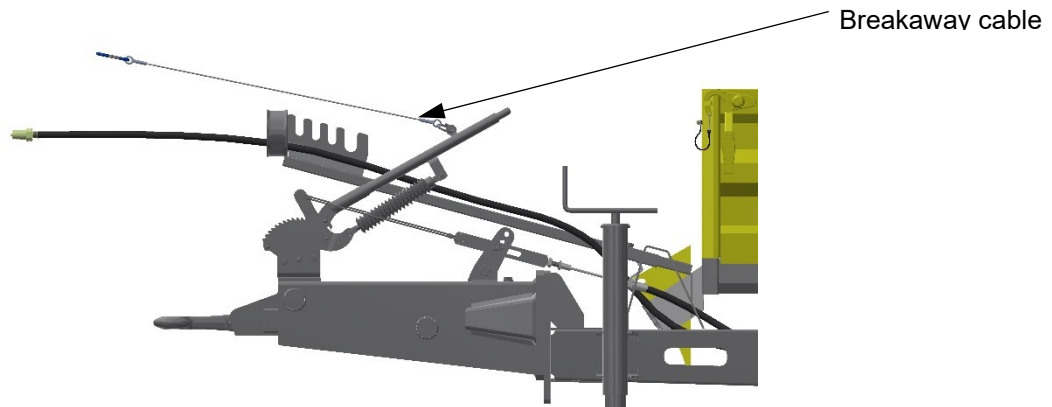


### Danger!

**All tasks on the brake system must be executed by trained specialists in qualified specialized workshops.**

This multifunction trailer is equipped with an overrun device with parking brake, automatic reverse, and breakaway cable.

The breakaway cable activates the handbrake, if the coupling between towing vehicle and trailer should release.



### Warning!

**After coupling the trailer to the towing vehicle, the breakaway cable must be connected to the towing vehicle.**

## 4.5 Chassis, tires

The chassis has a rigid, unsprung axle. The permissible axle load is 2500 kg.

The following tires are approved:

Tire dimensions		Air pressure
1	205 R 14C PR8 109 P	3.4 bar
2	10.0/75-15.3 PR10 123 A8/129 A6	5.0 bar

Table 2: Tire dimensions and tire pressure



### Warning!

**Ensure correct tire pressure and the legally prescribed tread depth**

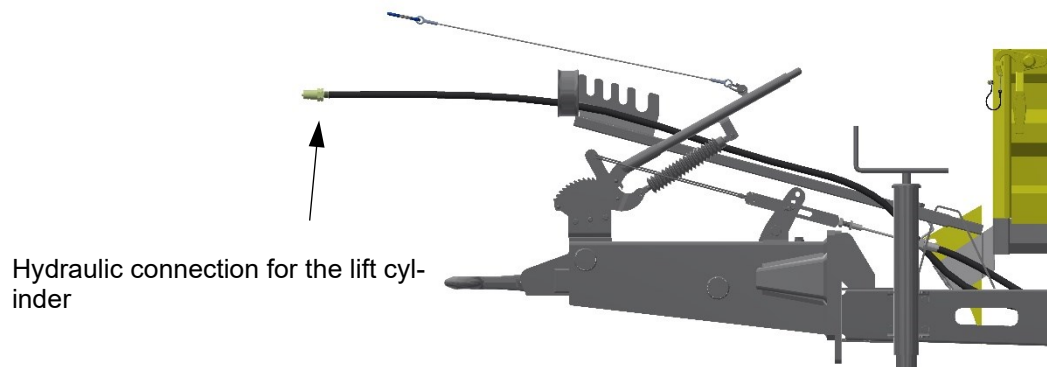
## 4.6 Dump device

The multifunction trailer is equipped with a hydraulic, single-acting lift cylinder. The lift cylinder is connected to the vehicle hydraulics. For this, there is a coupling on the rear of the vehicle, which you can operate from the driver seat with a control lever.

**Warning!**

The function of the hydraulic system is described in the manual of the towing vehicle. Comply with the instructions in the operating manual of the towing vehicle.

Lifting occurs through the oil pressure of the tractor. Lowering occurs through the empty weight.



Hydraulic connection for the lift cylinder

Fig. 14: Connecting the lift cylinder

**Danger!**

Do not attempt to find a possible leak in the hydraulic system with your fingers. Oil under high pressure can cause severe injuries.

**Note!**

A single-acting control valve is preferred, however in principle the trailer can also be connected to a double-acting control valve. In this case the hydraulic system will build up pressure when lowering the load platform, which can be indicated by increased engine noise.



Lift cylinder

Fig. 15: Lift cylinder

The complete dump device is already assembled in the delivery status of the multifunction trailer. Do not make any changes to the dump device. Note particularly, that you are not allowed to remove or extend the securing cable. The securing cable limits the lift of the dump device in all three directions. The length of the cable ensures that the possibility of the entire trailer tipping over when dumping the load bed, is excluded.



Leave the securing cable of the dump device in its original delivery condition. A securing cable that is too long can result in the entire trailer tipping over when dumping the load bed.

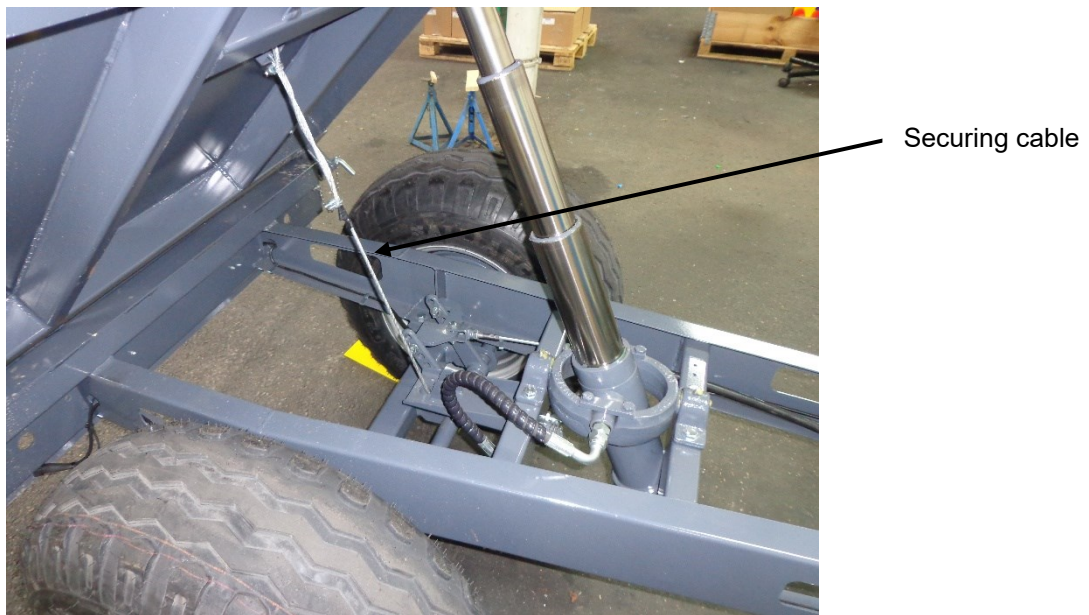


Fig. 16: Securing cable of the dump device

### 4.7 Panel height extension

The panel height extension is inserted into the stanchions of the base side panel. The side panels of the panel height extension are hooked on with pendulum mechanisms and are also locked through the locking mechanism of the base panels. Assembly instructions are provided with the panel height extension

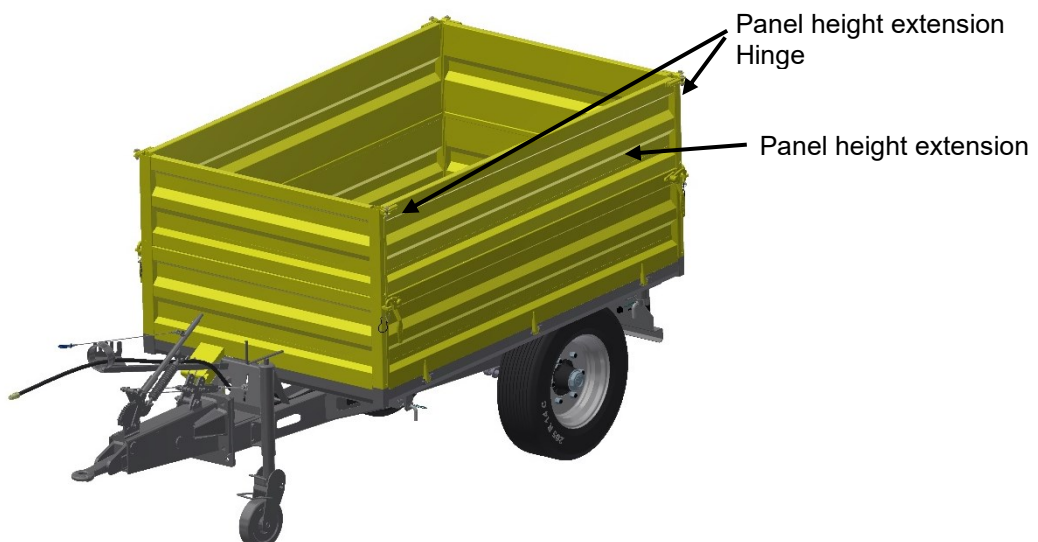


Fig. 17: Panel height extension

### 4.8 Grating height extension

The grating height extension is used to transport goods that can be blown away. The grating height extension is hooked on with a pendulum mechanism and is al-

so locked through the locking mechanism of the base panels. Assembly instructions are provided with the grating height extension.



Fig. 18: Grating height extension

### 5 Operation



#### **Attention!**

**Before first use of the multifunction trailer, comply with the instructions on safety and handling for operation of the entire trailer and the connection to the tractor.**

#### 5.1 Coupling



**You are only allowed to couple the trailer to a towed vehicle with Ø40 coupling in accordance with DIN 11025, DIN 11028 Form A, B, C, and DIN 74051 Part 1.**

Tongue load is the weight load that acts on the trailer hitch when coupling a trailer.



**The tongue load must be directed downwards. Do not exceed the maximum tongue load of 400 kg!**

**Only couple the trailer to the tractor, or uncouple the trailer from the tractor, when the trailer is empty.**

##### 5.1.1 Coupling to the towing vehicle

1. Use the support wheel to adjust the height of the coupling.
2. Couple the trailer to the tractor.
3. Use the crank to lift the support wheel approx. 5 cm.
4. With your foot, move the support wheel upward.
5. Connect the hydraulic line to the tractor's hydraulic system.
6. Plug the electrical plug into the tractor's socket outlet.
7. Hook the breakaway cable into a strong part of the tractor.

##### 5.1.2 Uncoupling from the towing vehicle

1. For uncoupling, ensure that you are on a flat, load-bearing and horizontal surface. When uncoupling, ensure that you do not disrupt road traffic.
2. Switch off the engine of the tractor and remove the ignition key.
3. Engage the parking brake and/or place the wheel chocks appropriately for the wheels.
4. Unplug the plug connector from the tractor's socket outlet.
5. Uncouple the hydraulic line of the trailer from the tractor.
6. Unhook the breakaway cable from the tractor.
7. Move the support wheel downward.
8. Crank the support wheel downward so that the coupling is offloaded.
9. Open the coupling and uncouple the trailer from the tractor.

#### 5.2 Brakes

This multifunction trailer is equipped with an overrun device with parking brake, automatic reverse, and breakaway cable.

- The trailer is automatically braked when the towing vehicle is braked and the shear forces act on the overrun hitch.
- The braking effect is not in force when driving in reverse.





**When the parking brake is engaged the trailer is only safeguarded from rolling forward. Unbraked, the trailer can roll rearward up to 20 cm until the full braking force is engaged. Safeguard the vehicle from rolling rearward with the wheel chocks.**

The breakaway cable must be hooked in on a stable, fixed part of the tractor. It is used to brake the trailer in the event of unintentional uncoupling from the towing vehicle while driving. Driving without the breakaway cable hooked in is prohibited. The loop of the breakaway cable must be of sufficient length that the cable is not tautened, even with extreme cornering and thus triggers emergency braking of the trailer.

Always secure the parked trailer with parking brake or the wheel chocks. Always use wheel chocks when parking on inclines.



**Do not use the parking brake in freezing temperatures or after longer standstill periods; the brake pads can seize. Use the wheel chocks instead.**

### 5.3 Driving

Each time before starting to drive, the driver must ensure for himself that the trailer and the load are in faultless condition.

The following checklist must be run through each time before driving:

- **Coupling** – is it properly locked in place and secured?
- **Breakaway cable** – is it hooked-in above the trailer hitch of the towing vehicle?
- **Handbrake** – is it disengaged?
- **Wheel chocks** – are they removed and safely stowed?
- **Electrical system plug connector** – is it firmly connected?
- **Hydraulic line** – is it connected?
- **Support wheel** – is it moved upward?
- **Panels** – are they swung up and locked?
- **Ball rocker bearing** – is the locking connector plugged in? Is it secured with a spring cotter pin?
- **Tarp** – is it securely fastened?
- **Tire pressure** – is it as legally prescribed?
- **Lighting** – is it functional?
- **Load** – is it properly distributed? Not overweight?

Driving instructions:

- Adapt the speed to road and weather conditions so that you can bring the tractor & trailer to a standstill at anytime with no problems.
- When the trailer is loaded, reduce speed on inclines so that you can bring the tractor & trailer to a standstill at any time with no problems.
- Bear in mind that steerability of the tractor can be impaired by the tongue load of the trailer. If necessary use weights on the front of the tractor.
- When driving with the trailer, bear in mind that the turning radius is greater.
- Bear in mind that on curves the trailer follows the tractor at a smaller radius.
- If the tractor & trailer start to fishtail, reduce speed and do not make extreme steering movements. Fishtailing is usually caused by faulty load distribution, insufficient tongue load or insufficient tire pressure.

- Bear in mind, on slick, slippery roads the driving behavior and braking behavior is worsened due to the reduced road grip of the tires.



**Danger!**

**When reversing with the trailer your view is limited.**

**When reversing, have an experienced person guide you to ensure that no one is in danger. The guiding person and the driver must always maintain visual contact.**

### 5.4 Loading



**Danger!**

**Comply with the following instructions when loading the trailer:**

- Only load the trailer when it is coupled to the tractor.
- When loading, the trailer must be on a flat, solid, and horizontal substrate.
- Pay attention to the distribution of the load. The load weight must be concentrated on the axle. Do not load the rear area of the trailer. Distribute the load so that there is a little more of the load in the front area than is in the rear area.
- Do not exceed the permissible gross weight.
- Do not exceed the maximum tongue load, see section 5.1. The tongue load must be at least 500 N (approx. 50 kg) in all dumping positions.
- The load must be secured against slipping. The load must be secured in such a manner that it does not slip or detach even in extreme driving situations (full braking, evasive maneuvers, etc). For bulk goods, leaves or grass, secure the load with a cover tarp or net.
- Use the eyes on the load bed for lashing.

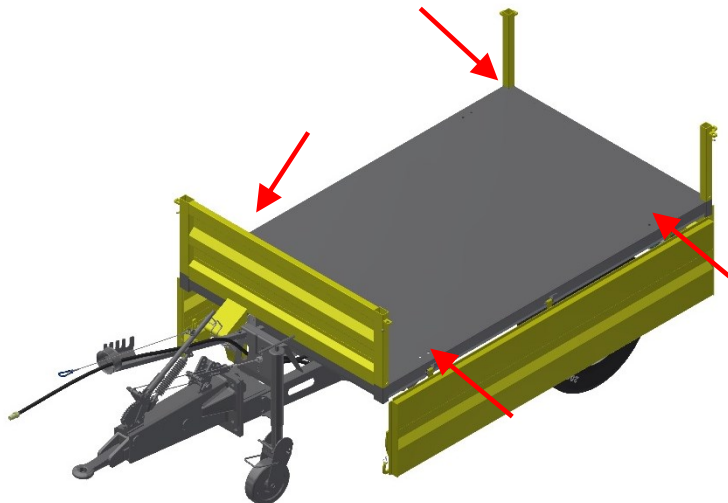


Fig. 19: Position of the lashing eyes

- The load must not project above the panels.
- Load in such a manner that the center of gravity is low.

The density of several frequently used loading goods in dry status is shown below. Avoid overloading the trailer.

- Grass, fresh      approx.0.2    0.3 kg/dm<sup>3</sup>
- Wood              approx.0.5    0.9 kg/dm<sup>3</sup>



- Sand, dry                      approx.1.4    1.6 kg/dm<sup>3</sup>
- Sandstone                    approx.2.1    2.5 kg/dm<sup>3</sup>
- Snow                            approx.0.1    0.6 kg/dm<sup>3</sup>
- Coal                             approx.1.35   kg/dm<sup>3</sup>
- Bricks                         approx.1.8    kg/dm<sup>3</sup>

## 5.5 Opening/closing the panels



### Danger!

Even when opening the panel the load can fall from the trailer. Never stand in front of the panel that will be opened.

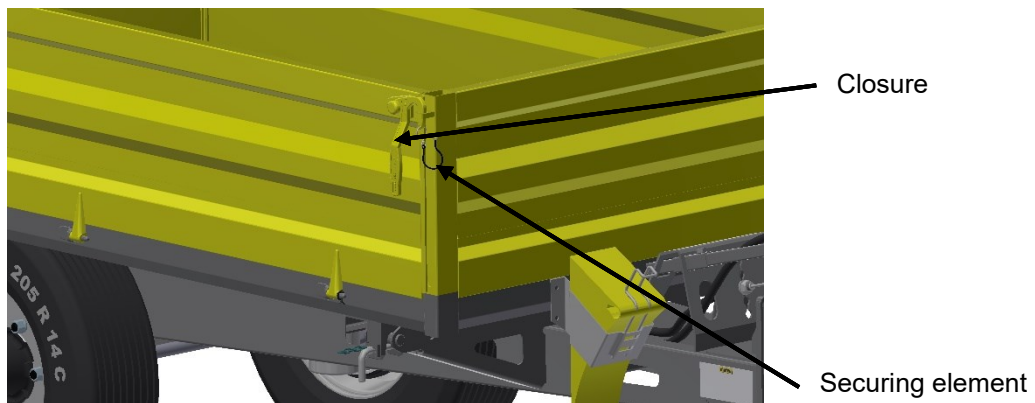


Fig. 20: Panel closure

### 5.5.1 Opening a panel

1. Remove the securing elements
2. Open the first latch



### Danger!

The load can force the panel open. Do not stand in front of the panel.

3. Open the second latch.
4. Swing the panel downward.



### Note!

The panel height extension will be automatically unlocked when the lower panel is unlocked.

### 5.5.2 Closing the panel

1. Swing the panel upward.
2. Close the first latch and secure it with the securing element.
3. Close the second latch and secure it in the same manner.

## 5.6 Dumping

You can dump the load bed laterally (left/right) or rearward.



**Dumping and lowering of the load bed must always be controlled from the driver seat. Ensure that no one is standing in the danger zone.**



**The locking bolts must never be inserted above the vehicle diagonal.**

Proceed as follows to dump the load bed.

1. Pull the spring cotters and locking pins of the tilt bearing on the side of the trailer to be lifted. For example, if you want to dump to the right, you must remove the locking pins on the left side of the trailer. If you want to dump rearwards, you must pull the locking pins on the front side of the trailer.
2. Open the panel on the side, on which you want to dump the load bed and carefully let down the panel.



Fig. 21: Load bed locking mechanism



**Maintain a safe distance from the danger zone.**  
**When opening the panel, ensure that the load material cannot cause personal injury or material damage by slipping or falling out.**

### 5.7 Support wheel

The support wheel is designed to only provide support when uncoupling the trailer from the tractor and for parking the trailer. Do not maneuver the trailer when the support wheel has been let down.

Operation of the support wheel is described in the section **4.3 Support wheel**.

## 6 Maintenance



**Failure to perform inspections or maintenance and cleaning tasks at the proper time or failure to perform them at all can result in trailer damage and thus can cause accidents. Moreover, your warranty claim will be invalidated.**

### 6.1 Cleaning and care

To assure retention of value and road worthiness of the trailer, you must check the multifunction trailer for contamination after each use and clean it if necessary. Clean the vehicle with water. For stubborn contamination you can also use a cleaning agent.

If possible avoid contact with salt, corrosive acids or fertilizer. Wash the trailer with aggressive media after each use.

Scratches or other paint damage must be repaired immediately.

Taillights and other lighting elements must always be intact, free, and clean. Wash or clean regularly.

### 6.2 Lubricating schedule

You must lubricate the multifunction trailer on a regular basis. If nothing to the contrary is specified, use all-purpose grease as specified in DIN 51825 KTA 3 K. Proceed in accordance with the following lubrication schedule:



**Execute maintenance tasks under the lifted load bed, only when the trailer is empty, and with the lowering safeguard of the load bed engaged. Switch off the tractor engine and remove the ignition key. Engage the parking brake of the tractor and of the trailer.**

- Clean the overrun brake after each use in an aggressive environment, however at least 2x a year. Use a lithium L3 grease
  1. Clean the overrun device
  2. Use a brush to lubricate the lubricating points (S)

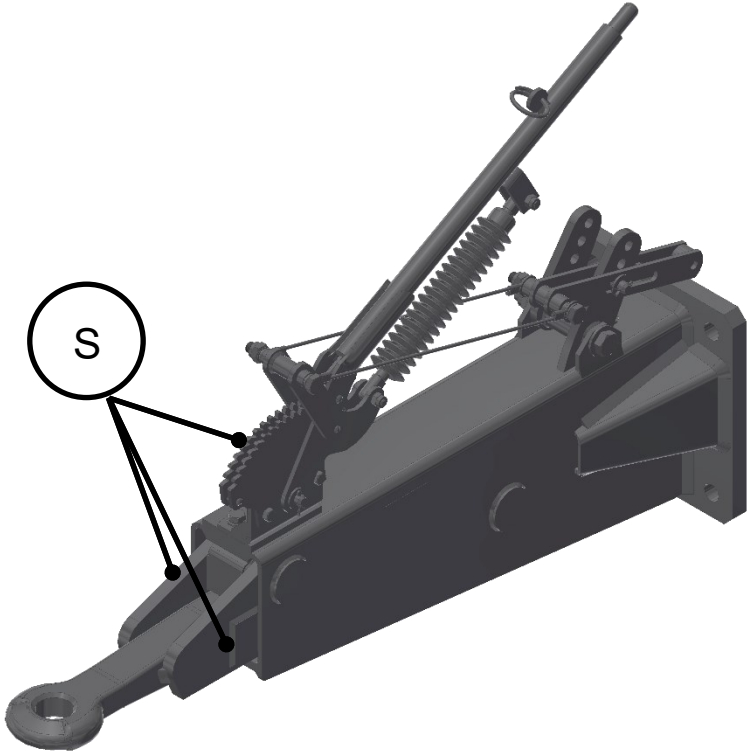


Fig. 22: Lubricate the overrun brake

- Clean and lubricate joints of the dumping mechanism twice a year.



Fig. 23: Lubricate the dumping mechanism



Fig. 24: Lubricate the dumping mechanism



Fig. 25: Lubricate the dumping mechanism





Fig. 26: Lubricate the dumping mechanism

- Lubricate the joints of the dumping cylinder with a grease gun.

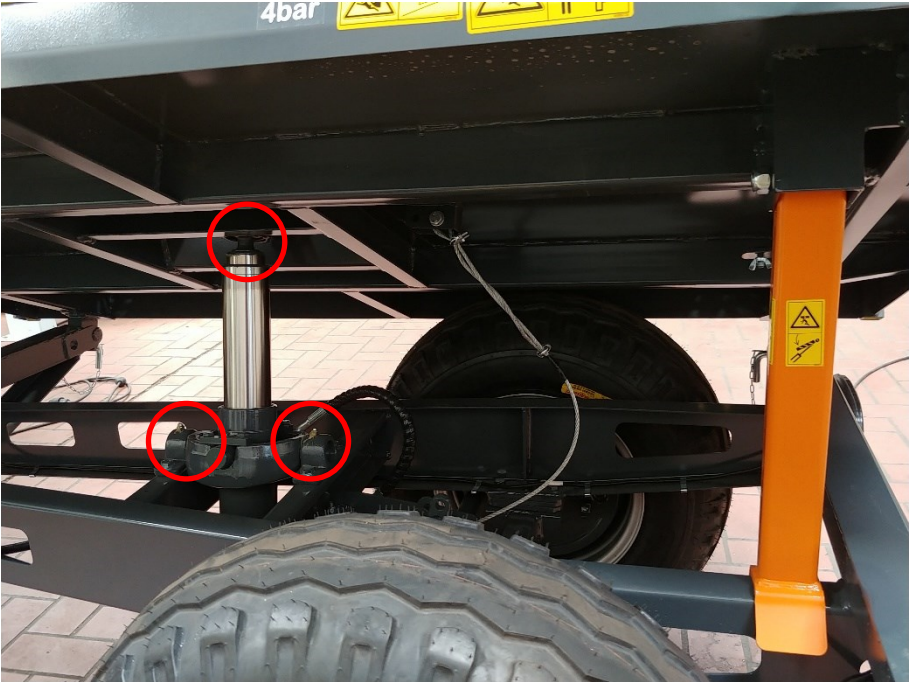


Fig. 27: Lubricate the cylinder

## 6.3 Wheels

Each time before driving, regularly check the tire pressure, tread depth of the tires, and tightening torque of the wheel studs. Ensure the following values:

- Tightening torque of the wheel studs:  $365 \pm 15$  Nm

### 6.3.1 Changing tires

1. Drive the trailer to a solid, flat and horizontal surface. Ensure that you do not obstruct road traffic.
2. Engage the parking brake.
3. Secure the trailer with the wheel chocks in both directions.
4. Slightly unscrew the wheel studs. Do not remove the wheel studs in this step.
5. Use a jack. Place the jack on the marked points under the axle.

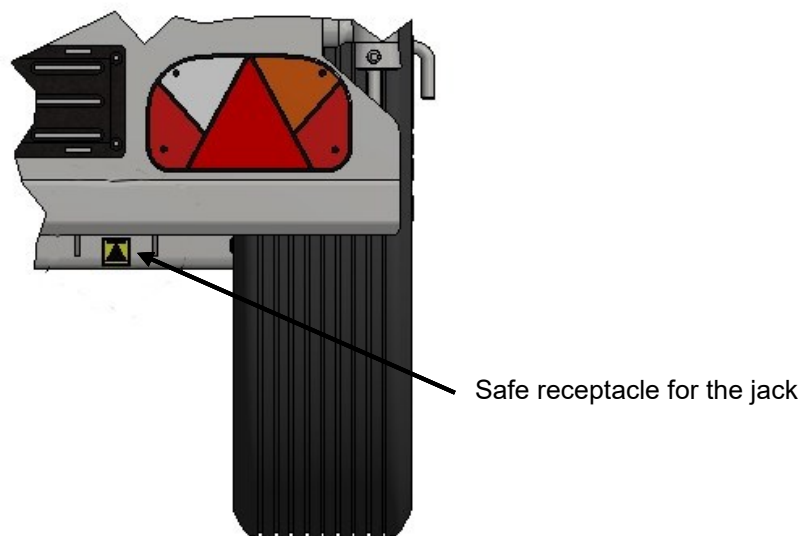


Fig. 28: Safe receptacle for the jack

6. Only lift the trailer high enough that the wheel is lifted off the ground. In this process, ensure that the axle is fully seated on the jack.



**Do not get under the trailer when it is lifted with the jack. Due to unforeseeable causes the trailer can fall down and cause severe injuries.**

7. Remove the wheel studs.
8. Pull the wheel off of the wheel hub.
9. Fit the new wheel on the wheel hub.
10. Tighten the wheel studs slightly.
11. Lower the jack and remove it from the trailer.
12. Tighten the wheel studs to the prescribed torque in a cross pattern.
13. Remove the wheel chocks and store them in the hook-in element intended for this purpose.
14. Release the parking brake.
15. Tighten the wheel studs to the prescribed torque once again after 50 km.

## 6.4 Cable pulls

Visually inspect all cable pulls twice a year. You must check the securing cable and the cable pulls of the wheel brakes for possible damage. If you detect dam-

age, you must have the damaged parts repaired in a specialized workshop without delay.

### 6.5 Electrical system, pin assignment

Your trailer is equipped with a 7-pin plug connector.



**Use an adapter if the plug connection of the towing vehicle does not match the plug connector of the trailer.**

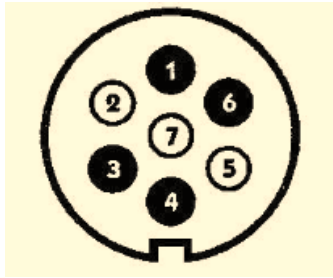


Fig. 29: Plug connector pin assignment

Pin	DIN no.	Consumer	Cable color
1	L	Indicator left	Yellow
2	54g	Not assigned	Blue
3	31	Ground	White
4	R	Right indicator	Green
5	58R	Taillight right	Brown
6	54	Brake light	Red
7	58L	Taillight left	Black

Table 3: Plug connector pin assignment



## 7 Maintenance

Note: All inspections must be executed in the facilities of specialized companies.

### 7.1 Axle and brakes

Maintain the axles as specified in the original maintenance manual provided by the axle manufacturer. The maintenance manual is provided with your trailer. If this is not the case, then contact matev GmbH and provide us with the serial number of the trailer.

An excerpt of the maintenance manual concerning the time intervals for maintenance is provided below.





Übersicht über die Schmierung und Wartung der Starr- und Lenkachsen	Nach den ersten unter Vollast gefahrenen 10 km.	Nach den ersten 200 Betriebsstunden oder nach den ersten gefahrenen 300 km.	Alle 500 Betriebsstunden oder alle gefahrenen 8500 km. <sup>1</sup>	Alle 1500 Betriebsstunden oder alle gefahrenen 25000 km. <sup>1</sup>	Alle 3000 Betriebsstunden oder alle gefahrenen 50000 km. <sup>1</sup>
 Schmierung  Wartung					
<b>Schmierung</b> mit Spezialfett <b>ADR Lithogrease 3</b>					
A – Schmierfett in den Nabenlagern erneuern					
B – Bremsnockenhalterungen					
C – Bremshebel					
D – Lenkstangengelenke					
E – Lenkscharniere					
<b>Wartung</b>					
1 - Anzugsmoment der Radmuttern kontrollieren					
2 – Bremsbeläge kontrollieren					
3 – Lagerspiel kontrollieren und gegebenenfalls einstellen					
4 – Bremshebelweg kontrollieren und gegebenenfalls einstellen					
5 – Lenkscharniere auf Spiel kontrollieren und gegebenenfalls einstellen					
6 – Anzugsmomente der Stoßdämpferschrauben kontrollieren					
7 – Anzugsmomente der Befestigungsschrauben des Steuerzylinders der Lenkung kontrollieren					
8 – Anzugsmomente der Lenkstangenschrauben kontrollieren und Lenkwinkel einstellen					
9 – Anzugsmomente der Schrauben der Bremszylinderhalterungen kontrollieren					

Fig. 30: Excerpt from the original maintenance manual for axles and brakes

## 7.2 Inspection

At vehicle handover, then repeat every year

Vehicle assembly	To check	Work to be executed
Safety device (-> section 3.5)	In accordance with section 3.5 Safety notices and safety devices	Adjust or replace if necessary.
Brake system	In accordance with section 7.1 Axle and brakes	Adjust or replace if necessary.
Hydraulic lines	Check for damage, leaks.	Replace if necessary, retighten fittings.
Tires	Check air pressure	Adjust if necessary
Tires	Visual inspection	Replace if damaged or porous
Tires	Tread depth at least 1.6 mm	Replace if less than 1.6 mm
Tires	Check for concentricity	Balance if necessary
Body	Hinges	Lubricate
Lighting	Lamps	Repair if necessary
Lighting	Plug and socket connections	Replace if necessary
Lighting	Cables	Replace if necessary
Reflectors	Visual inspection	Replace if damaged
Overrun device	Lubricating nipple	Lubricate
Overrun device	Cable pulls	Replace if damaged
Overrun device	Cable pulls	Lubricate cable guide
Overrun device	Screw connection	Replace if necessary
Entire trailer	Paint damage	Repaint, if necessary

## 7.3 Repaint if necessary

1/4 year after transfer

Vehicle assembly	To check	Work to be executed
Safety device (-> section 3.5)	In accordance with section 3.5 Safety notices and safety devices	Adjust or replace if necessary.
Brake system	As specified in section 7.1 Axle and brakes	Adjust or replace if necessary.
Hydraulic lines	Check for damage, leaks.	Replace if necessary, retighten fittings.
Tires	Check air pressure	Adjust as specified in Table 2
Body	Hinges	Lubricate
Lighting	Check	Repair if necessary
Entire trailer	Paint damage	Repaint, if necessary

8 Proof of inspection

Tasks to be executed as specified in section 7 Maintenance

	Stamp	Date	Signature
Transfer inspection			
Repaint if necessary 1/4 year after transfer			
1. Annual inspection			
2. Annual inspection			
3. Annual inspection			
4. Annual inspection			
5. Annual inspection			
6. Annual inspection			

Proof of inspection

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	Stamp	Date	Signature
7. Annual inspection			
8. Annual inspection			
9. Annual inspection			
10. Annual inspection			
11. Annual inspection			
12. Annual inspection			
13. Annual inspection			
14. Annual inspection			
15. Annual inspection			

	Stamp	Date	Signature
16. Annual inspection			
17. Annual inspection			
18. Annual inspection			
19. Annual inspection			
20. Annual inspection			
21. Annual inspection			
22. Annual inspection			
23. Annual inspection			
24. Annual inspection			

### 9 Repair

If there are faults, problems, or other indications of malfunction, contact your sales consultant or contact the manufacturer directly:

matev GmbH

Nürnberger Str. 50  
90579 Langenzenn, Germany

Tel.: +49 9101 9087-0

[www.matev.eu](http://www.matev.eu)

[info@matev.eu](mailto:info@matev.eu)

Also comply with the following instructions:

- Repairs must only be undertaken in the facilities of specialized companies.
- Only original spare parts from matev must be used for repairs.
- No modifications are permitted on the trailer.

### 10 Disposal

The trailer must be disposed of in accordance with the applicable regulations of the municipality or the country.

Take the parts to the collection points for residual waste, special waste, or recycle them depending on material.

matev GmbH does not provide any disposal services.

### 11 Warranty

The general terms and conditions of matev GmbH apply.

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## 14 EG – Declaration of Conformity

EC Declaration of Conformity for a machine to confirm compliance with Directive 2006/42/EC and with the statutory regulations issued for its implementation.

The manufacturer     matev GmbH  
                                 Nürnberg Str. 50  
                                 90579 Langenzenn, Germany

declares that the machine TRL-3D 20

complies with the provisions of the Machinery Directive 2006/42/EC and with the implementing national statutory regulations.

The signer is authorized to compile the technical documents.

The address is the address of the manufacturer.

Date / Signature January 2020

Name of signer

Michael Volz

Signer information

Managing Director